

MASON COUNTY ROAD COMMISSION

Board of Commissioners

Douglas Robidoux, Chairman

Bill Schwass, Vice-Chairman

Nick Matiash, Member

Wayne A. Schoonover, P.E., Manager/Director

510 E. State St.

PO Box 247

Scottville MI 49454-0247

Office: (231) 757-2882 Fax: (231) 757-2662

e-mail: info@masoncountyroads.com

NOTICE TO BIDDERS

The Board of County Road Commissioners of Mason County will receive sealed bids at their office at 510 E. State St., Scottville MI 49454, until 11:00 a.m. on Tuesday, June 19, 2018, for the following:

US-10 CATCH BASIN REPAIRS/REPLACEMENT

Specifications and bid forms may be obtained at the Road Commission offices during regular business hours or by visiting our website at www.masoncountyroads.com.

All proposals must be submitted on Road Commission furnished bid forms. Said forms must be in sealed envelopes, plainly marked as to item bid and shall bear the name of the bidder.

The Mason County Road Commission hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprise will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of sex, race, color or national origin in consideration for an award.

The Board reserves the right to accept or reject any or all bids, to waive any irregularities in the bids and to make award in any manner they deem to be in the best interest of Mason County.

BOARD OF COUNTY ROAD COMMISSIONERS

DOUG ROBIDOUX, CHAIRMAN

BILL SCHWASS, VICE-CHAIRMAN

NICK MATIASH, MEMBER

PROPOSAL

US-10 CATCH BASIN REPAIRS/REPLACEMENT

**To: Board of Mason County Road Commission
510 East State Street
P.O. Box 247
Scottville, Michigan 49454-0247**

Gentlemen:

The undersigned Michigan Department of Transportation (MDOT) approved Ea (Drainage Structures) contractor submits the following prices for drainage structure repair/replacement on US-10 State Trunkline. MDOT 2012 Standard Specifications shall apply, as modified herein.

A. BID ITEMS

REFER TO ATTACHED BID SPREAD SHEET, WHICH SHALL BE INCLUDED AS PART OF THIS BID PROPOSAL

B. PAYMENT TERMS:

Payment within 30 days of receipt of invoice if no issues.

C. Completion Date: All work shall be completed on or before September 20, 2018. The Mason County Road Commission reserves the right to add or delete bid quantities. MCRC has the right to cancel the contract at any time if contractor performance is unsatisfactory.

Submitted by: _____

Address: _____

Email: _____

Office Phone: _____ Cell: _____

Signature: _____ DATE _____

Bids shall be in a sealed envelope, plainly marked as to contents. The right is reserved by the Mason County Road Commission, to reject any and all bids, and to accept the bid that is in the best interest of the County.

Closing Date 11:00 am, TUESDAY, June 19, 2018

US-10 CATCH BASIN REPAIRS/REPLACEMENT

STRUCTURE NUMBER	STRUCTURE CONDITION	STATION	FIX	SLOT		VIDEO COMMENTS	LOCATION	REPAIR COST	UNIT
				DETAIL	DRAINS				
1A - 1B	Poor	159+90 LT	Informational Quantities Only:				500' east of Brye Rd, N Side	\$	LSUM
					90 Syd				
					2 Ea				
					205 Cyd				
					2 Ea				
					165 Cyd				
					45 Cyd				
					90 Syd				
					35 Syd				
					5 Ton				
					35 LF				
6	Good	166+00 RT	2' Adj, 10' C&G	1R	No	12" N, SE, Clean 4" E 1/2 Full Debris	In front of #3815 (Staples)	\$	LSUM
14	Good	172+75 RT	2' Adj, 10' C&G	1R	No	12" N, 1/2 full sediment/debris	Opposite driveway to #3736 (Chevy Dealer)	\$	LSUM
32	Good	187+75 LT	2' Adj, 10' C&G	1L	No	12"S Clean, 12" NE 1/3 full sediment	690' east of Dennis Rd, opposite #3473	\$	LSUM
47	Good	199+25 RT	1' Adj, 10' C&G	1R	No	12" N&E, timber in pipe, no sediment	In front of, east of #3386 (NAPA)	\$	LSUM
49	Good	199+30 RT	1' Adj, 10' C&G	1R	No	12"W, Timber in pipe	In front of, east of #3386 (NAPA)	\$	LSUM
52	Good	201+70 RT	1' Adj, 10' C&G	1R	No	12"N, Clean	400' east of #3386 (NAPA)	\$	LSUM
55	Poor	204+30 RT	2' Adj, 10' C&G	1R	No	12"N - No Video	2300' east of Dennis Rd	\$	LSUM
58	Poor	206+90 RT	Inst new structure, 1' Adj, 10' C&G	2R	No	12"N, 1/2 full sediment	920' east of #3386 (NAPA)	\$	LSUM
61	Good	209+50 RT	1' Adj, 10' C&G	1R	No	12"N, 1/3 full sediment	1195' east of #3386 (NAPA)	\$	LSUM
67	Good	215+50 RT	2' Adj, 10' C&G	1R	No	12"N, SE 1/2 full water	Between #3095 and #3045 west of Ford Dealer	\$	LSUM
81	Good	227+50 RT	2' Adj, Needs new casting	2R	No	12"N, 1/2 full debris	In front of #2980	\$	LSUM
95	Poor	245+50 RT	Inst new structure w/ new casting	2R	No	12" N, 1/2 full debris	In front of #2601 (Padnos Scrap Metal)	\$	LSUM
158	Poor	296+50 RT	Repl Struct, 10'x20' HMA Shldr	2R	No	12" N, 1/3 full sediment	1,040' East of Quarterline Rd	\$	LSUM
167	Good	307+50 RT	1' Adj, 10' C&G	1R	No	12" N, 1/2 full debris	65' east of Mt Vernon Dr	\$	LSUM
170	Poor	310+20 RT	inst new structure w/ new casting	2R	No	12" N, 1/2 full sediment	340' east of Mt Vernon Dr in front of #1655	\$	LSUM
173	Poor	312+20 RT	inst new structure, 25' C&G	1R	No	12" N, 1/2 full sediment	85' west of Amber Rd	\$	LSUM
183	Poor	316+30 RT	Inst new structure, 35' C&G	1R	Yes	12" N, 1/3 full sediment	330' east of Amber Rd	\$	LSUM
190	Good	324+00 RT	2' Adj, 10' C&G, Needs new casting	1R	No	12" N, Clean	1100' east of Amber Rd	\$	LSUM
194	Poor	326+20 RT	Repl Struct, 10'x20' HMA Shldr	2R	Yes	12" S, Clean	1320' east of Amber Rd	\$	LSUM
196	Good	331+30 RT	1' Adj, 10' C&G	1R	No	12" N, 1/2 full sediment	800' west of Gordon Rd	\$	LSUM
202	Poor	336+50 RT	Inst new structure, 10' C&G	2R	No	12" N, 1/2 full sediment	290' west of Gordon Rd	\$	LSUM
250	Poor	369+00 LT	Repl Struct, 10'x20' HMA Shldr	2R	No	Surface pavement failing - No Video	1620' west of US-31 North, in front #406	\$	LSUM
253	Poor	369+00 LT	Repl Struct, 10'x20' HMA Shldr	2R	No	Surface pavement failing - No Video	1620' west of US-31 North, in front #406	\$	LSUM
260	Good	374+75 RT	1.5' Adj, 10' C&G	1R	No	12" N, Clean	1045' west of US-31 North	\$	LSUM
						Traffic Control		\$	LSUM
Estimate Total								\$	

US-10 Catch Basin Repairs/Replacement

INSTRUCTIONS TO BIDDERS

1. **CONTRACT DOCUMENTS**

It is the intent of these contract documents to be as clear, complete, and consistent as possible.

2. **BIDDER INVESTIGATION**

The Bidder will be responsible for inspecting the various roads scheduled for paving and to determine all conditions under which he will be obligated to work. A copy of the projects and their location is provided to assist the Bidder in his investigation.

3. **PROPOSAL PREPARATION**

The name and legal status of the bidder, corporation, partnership or an individual shall be stated in the proposal. A corporation bidder shall name the state in which its Articles of Incorporation are held and must give the Title of the officer having authority, under the by-laws, to sign contracts. A partnership bidder shall give the full names and addresses of all partners. Anyone signing a proposal as an agent of another, or others, must submit with this proposal legal evidence of his authority to do so. The place of residence of each bidder, or the office address in case of a firm or company, with county and state, must be given after a signature.

The proposal shall be carefully prepared in strict accordance with these instructions, otherwise the bid may be rejected. The proposal for the work is on a unit price basis. All unit prices shall be completed in ink and the proposal legally signed in ink.

The unit prices stated in the proposal shall be plainly written. Illegibility of any work or figure in the proposal may be sufficient cause for rejection of the proposal.

Each proposal must be enclosed in a sealed envelope, addressed to the Mason County Road Commission, and labeled "US-10 Catch Basin Repairs/Replacement."

4. **BID SURETY**

A Bid Bond or Certified Check must accompany this bid in an amount not less than five percent (5%) of the amount of the total bid, as based on quantities appearing on the form of the Proposal or other bid security as shall be outlined in the Proposal and/or the Special Provisions section of the Specifications. The bid bond or other security of the successful bidder will be held until the Contract has been duly executed.

5. PERFORMANCE BONDS AND INSURANCE

The successful bidder shall furnish performance and payment bonds in amounts equal to the contract bid price in such form and with such sureties licensed to conduct business in the State of Michigan.

6. CHAPTER 11 PROCEEDINGS

The Commission reserves the right to waive consideration of any bid submitted by a bidder who either has or has pending the filing of a Chapter 11 proceedings.

7. INSURANCE

Upon award of this contract, and prior to the performance of any work for the Mason County Road Commission, the contractor and their subcontractors shall furnish proof of insurance certificates with the following minimum requirements.

Worker's Compensation	Statutory Coverage
Bodily Injury and Property Damage	General Liability
	Each Occurrence \$1,000,000
	Aggregate \$2,000,000
Bodily Injury Liability and Property Damage Liability	Automobile
Bodily Injury Liability	Each Occurrence \$1,000,000
	Each Person \$ 500,000
Property Damage Liability	Each Occurrence \$1,000,000
Combined Single Limit for Bodily Injury and Property Damage Liability	
Owner's Protective Liability, including Contractual Liability	

Where required as an incident to compliance with Federal Laws and Regulations, bodily injury and property damage protection shall be extended to the Michigan Department of Transportation and the MCRC; and, where indicated by the identity of the contracting parties, the protection shall be extended to all participating political subdivisions and public corporations.

Contractors shall save harmless and indemnify the MCRC, as well as its Board of Road Commissioners, Administrators and Employees, against all claims for damages to public or private property and for injuries to persons arising out of and during the progress and to the completion of work.

The certificate of insurance must be submitted within ten days upon notification of award of Contract and will include and be named, in writing: "As additional insured the Mason County Road Commission, its Board of Road Commissioners, Administrators and Employees".

8. TESTING

All materials shall be obtained from MDOT approved sources. Testing for compliance with 2012 MDOT Standards and Specifications shall be the responsibility of the Contractor, who shall provide copies of all tests done.

MCRC shall have performed Independent Assurance Tests (IAT) and split samples, at their cost.

In the event the IAT shows the material to be out of specification the split sample shall be sent to an independent lab for testing. The split sample result shall govern, if it agrees with the IAT the penalty shall be in accordance with MDOT Standard Specifications; if the split sample agrees with Contractors, no penalty.

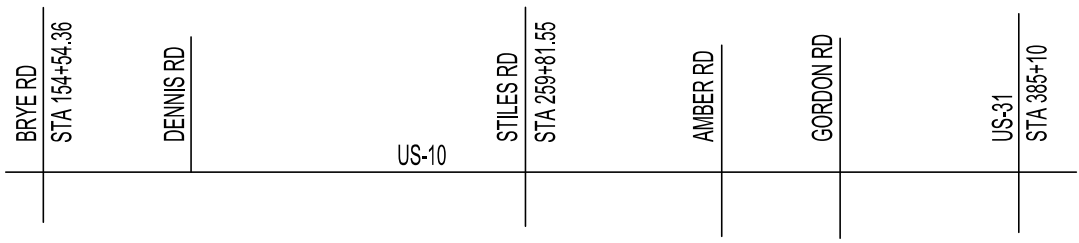
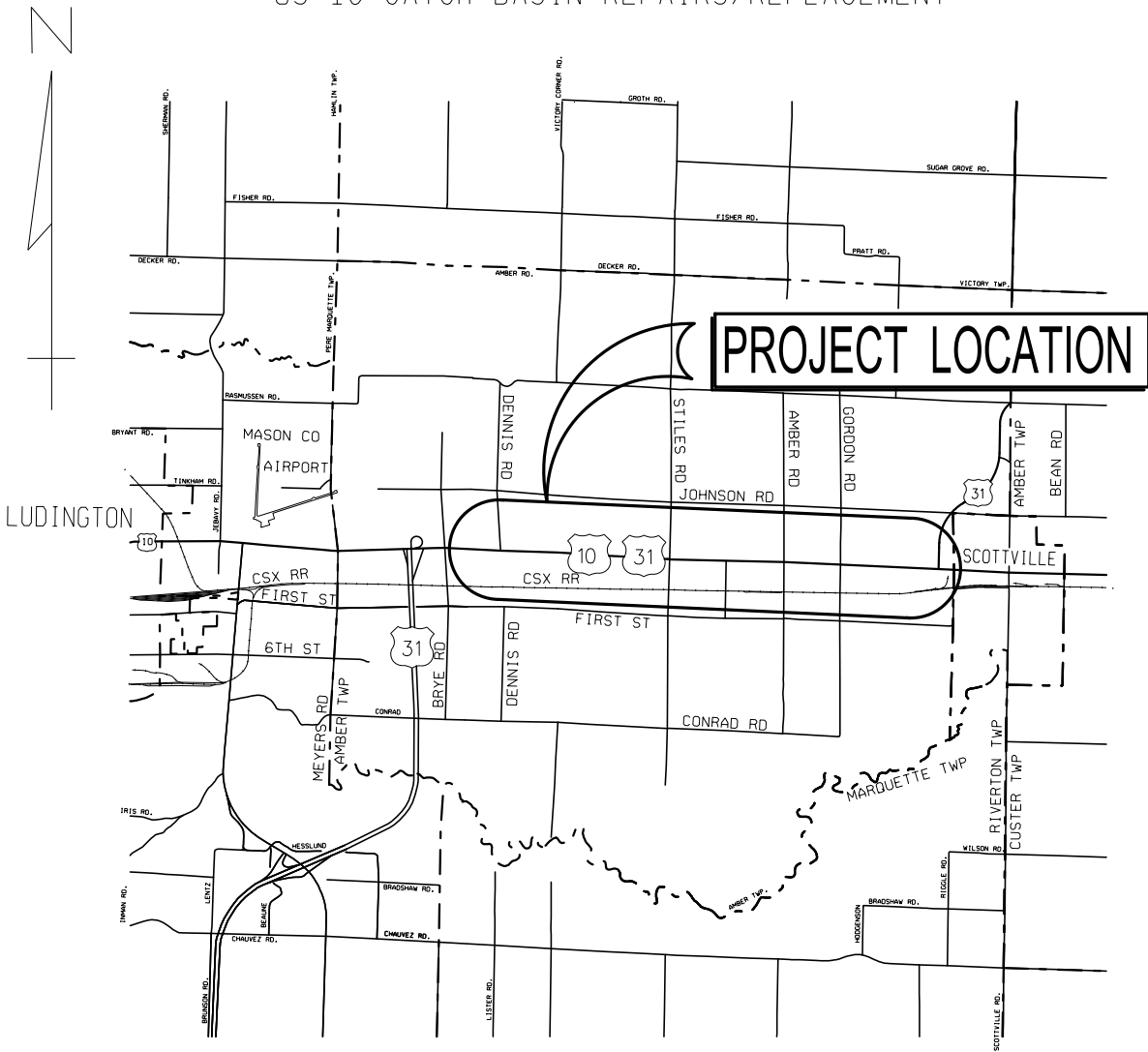
MICHIGAN DEPARTMENT OF TRANSPORTATION

US-10 CATCH BASIN REPAIRS/REPLACEMENT

DATE: 5/24/18

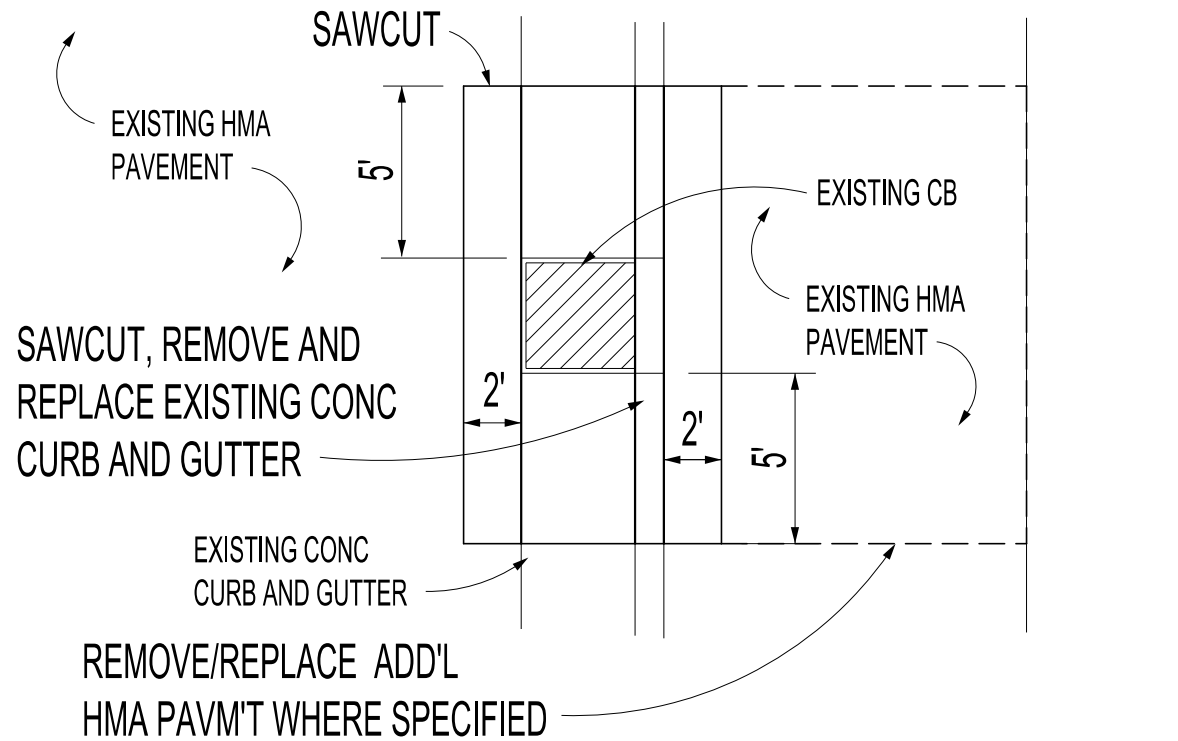
LAST CORRECTION BY:

FILE NAME: US10 TITLE.dgn

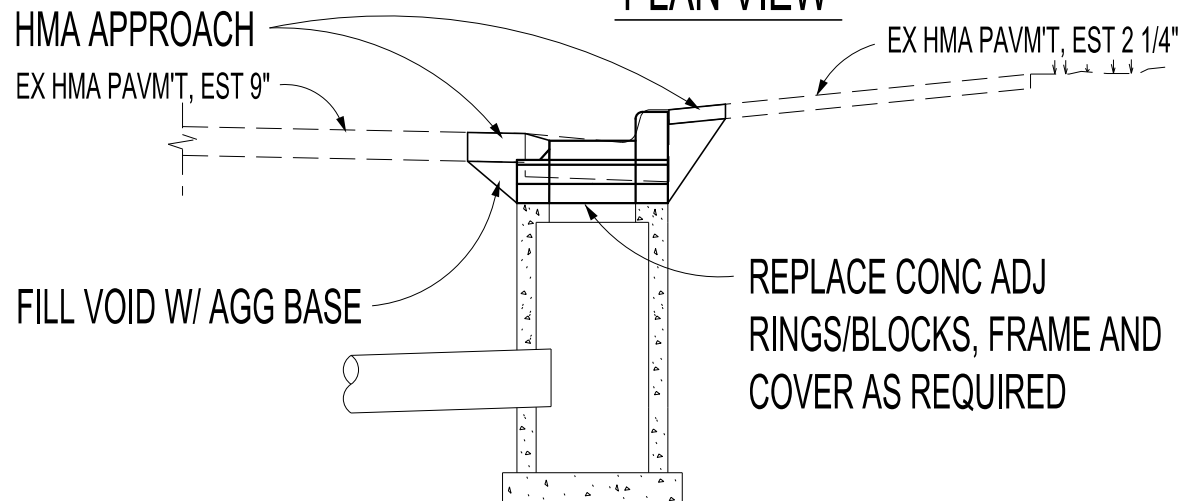


INTERSECTION PLAN

LOCATION MAP				
DATE 5/24/18	SCALE 1"=NA	CONT. SEC. 53032	JOB NO.	SHEET NO.



PLAN VIEW



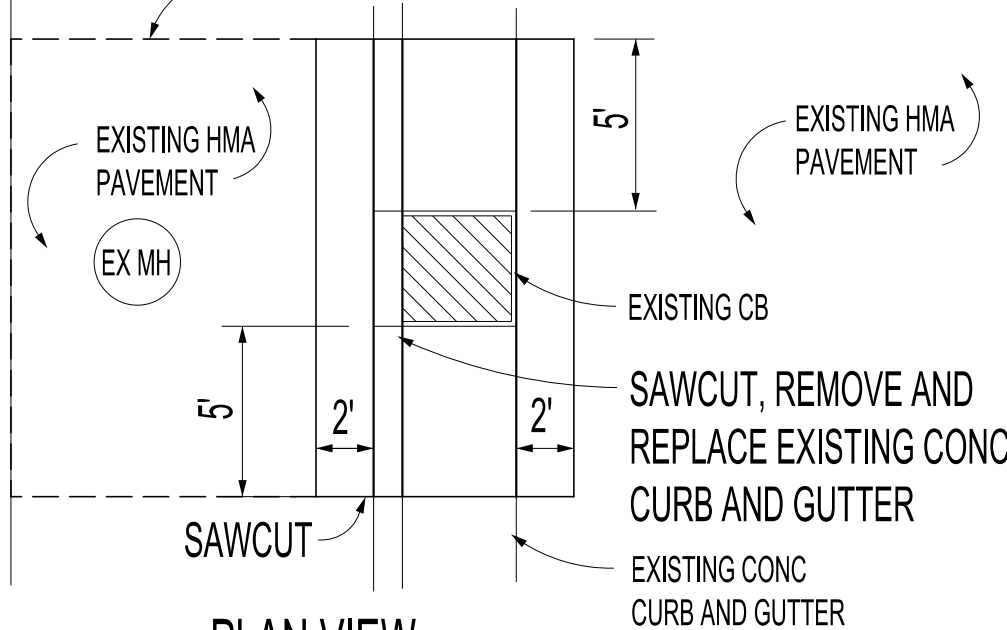
SECTION VIEW

NOTES:

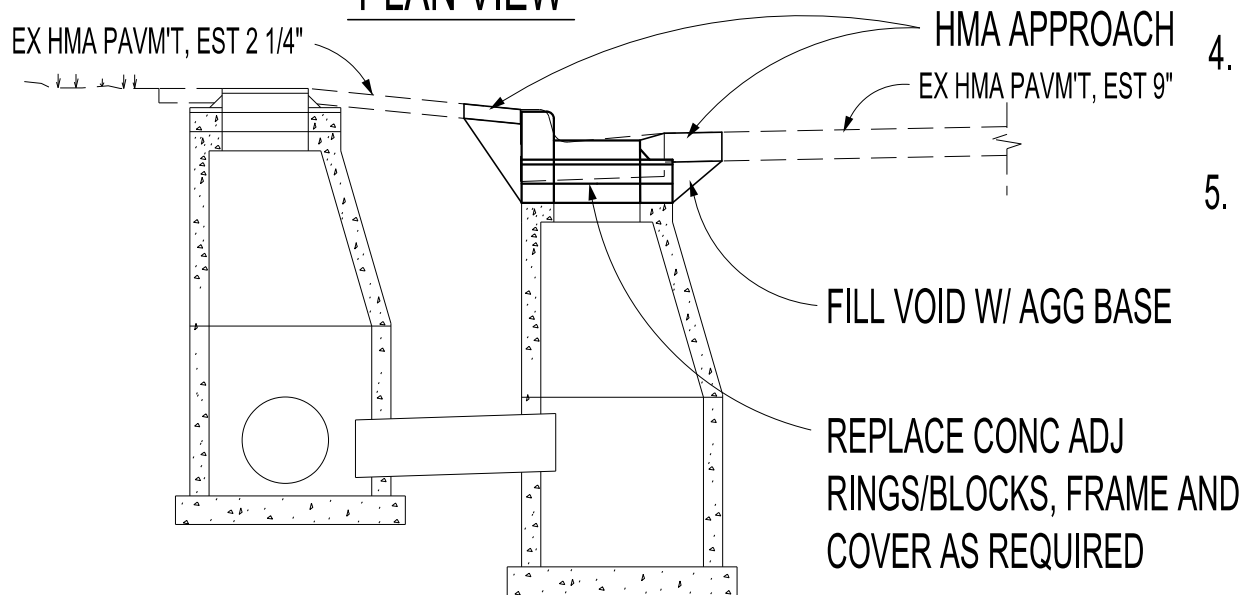
1. THE LIMITS FOR SAWCUTTING EXISTING CURB AND GUTTER ARE FOR REFERENCE ONLY, IF THE SAWCUT OCCURS NEAR A JOINT SAWCUT AT THAT JOINT LINE.
2. NEW CONC CURB AND GUTTER SHALL MATCH EXISTING.
3. AGGREGATE MAT'L SHALL BE COMPACTED TO PROPER SPECS PRIOR TO AND PAVING.
4. HMA PATCHING THICKNESS TO MATCH EXISTING PAVEMENT THICKNESS.
5. REFER TO AS-BUILT DRAWINGS FOR MORE INFORMATION PERTAINING TO STORM SEWER.

	NO SCALE	DATE: 4/2/17	CS: 53032	CATCH BASIN REPAIR TYPE #1R	DRAWING	SHEET
		DESIGN UNIT: FREDRICKSON	JN:	US-10 DRAINAGE STRUCTURE REPAIRS		
FILE:		TSC: MUSKEGON		BRYE ROAD TO WCL SCOTTVILLE		SECT 2

REMOVE/REPLACE ADD'L HMA PAVM'T WHERE SPECIFIED



PLAN VIEW



SECTION VIEW

NOTES:

1. THE LIMITS FOR SAWCUTTING EXISTING CURB AND GUTTER ARE FOR REFERENCE ONLY, IF THE SAWCUT OCCURS NEAR A JOINT SAWCUT AT THAT JOINT LINE.
2. NEW CONC CURB AND GUTTER SHALL MATCH EXISTING.
3. AGGREGATE MAT'L SHALL BE COMPACTED TO PROPER SPECS PRIOR TO AND PAVING.
4. HMA PATCHING THICKNESS TO MATCH EXISTING PAVEMENT THICKNESS.
5. REFER TO AS-BUILT DRAWINGS FOR MORE INFORMATION PERTAINING TO STORM SEWER.

1L



NO SCALE

DATE: 4/2/17

CS: 53032

CATCH BASIN REPAIR TYPE #1L

DRAWING SHEET

DESIGN UNIT: FREDRICKSON

JN:

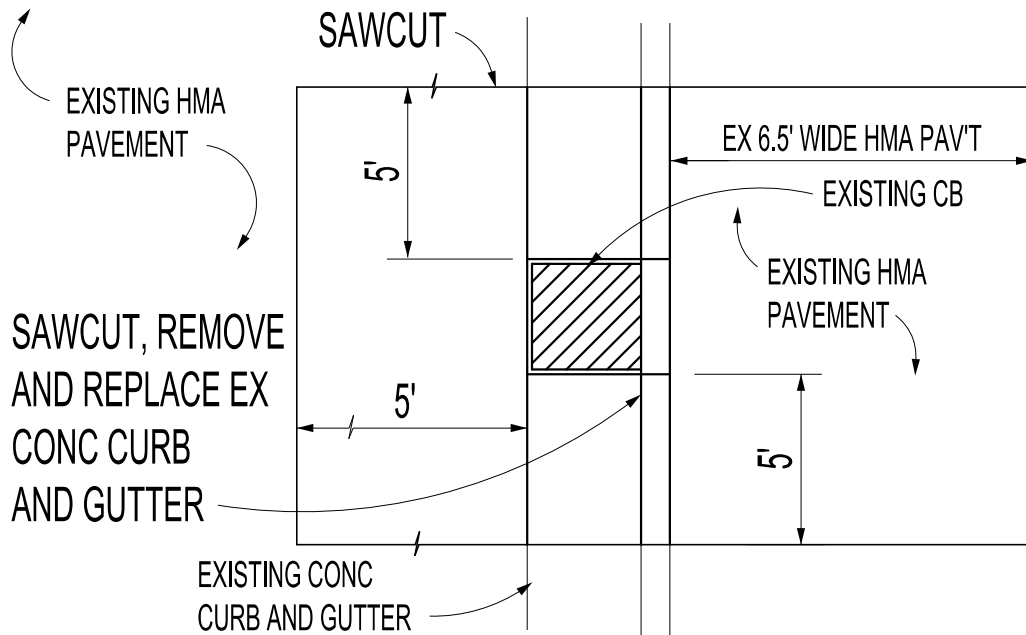
US-10 DRAINAGE STRUCTURE REPAIRS

SECT 2

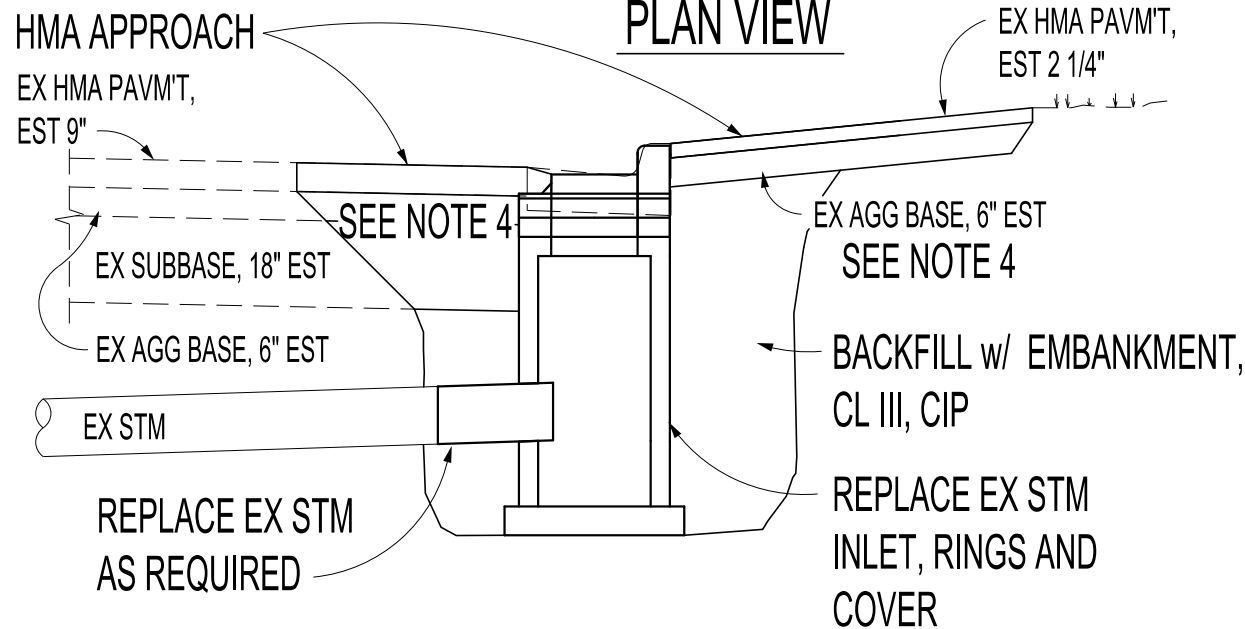
TSC: MUSKEGON

BRYE ROAD TO WCL SCOTTVILLE

FILE:



PLAN VIEW



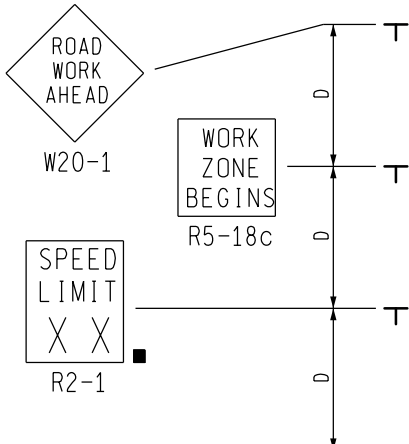
SECTION VIEW

NOTES:

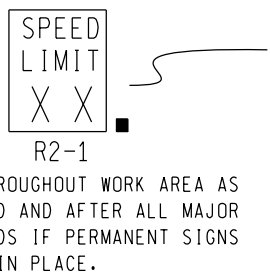
1. THE LIMITS FOR SAWCUTTING EXISTING CURB AND GUTTER ARE FOR REFERENCE ONLY, IF THE SAWCUT OCCURS NEAR A JOINT SAWCUT AT THAT JOINT LINE.
2. NEW CONC CURB AND GUTTER SHALL MATCH EXISTING.
3. AGG AND SUBBASE MAT'L SHALL BE COMPACTED TO PROPER SPECS PRIOR TO AND PAVING.
4. EX SUBBASE, AGGREGATE BASE AND PAVEMENT THICKNESS SHALL MATCH EXISTING.
5. REFER TO AS-BUILT DRAWINGS FOR MORE INFORMATION PERTAINING TO STORM SEWER.

2R

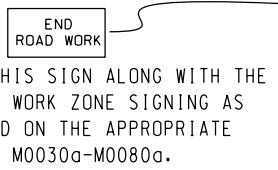
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		DESIGN UNIT: FREDRICKSON	JN:	US-10 DRAINAGE STRUCTURE REPAIRS		SECT 2
FILE:		TSC: MUSKEGON		BRYE ROAD TO WCL SCOTTVILLE		



NO SPEED REDUCTION THIS DIRECTION



PLACE THROUGHOUT WORK AREA AS INDICATED AND AFTER ALL MAJOR CROSSROADS IF PERMANENT SIGNS ARE NOT IN PLACE.

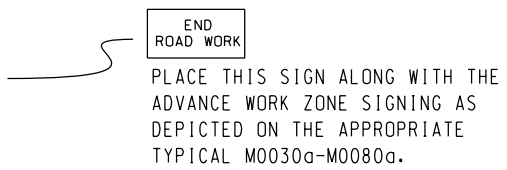
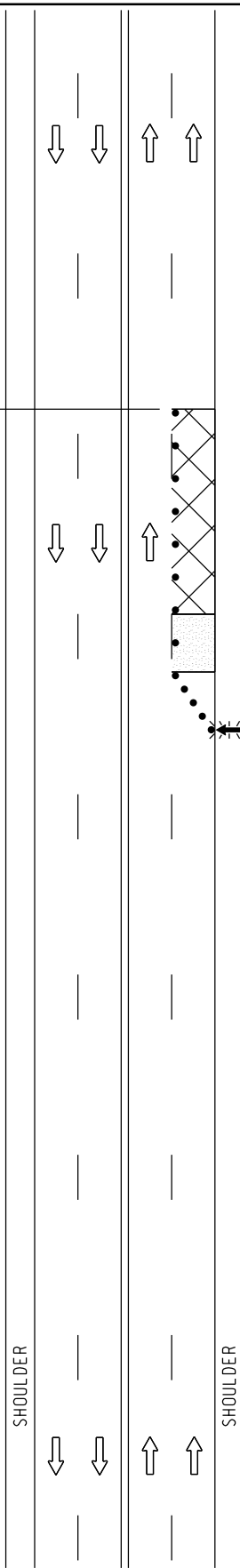


PLACE THIS SIGN ALONG WITH THE ADVANCE WORK ZONE SIGNING AS DEPICTED ON THE APPROPRIATE TYPICAL M0030a-M0080a.

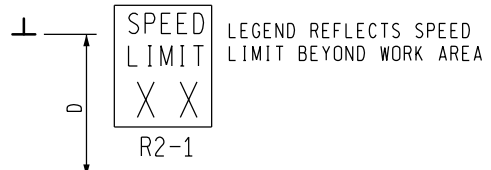
KEY

- • • CHANNELIZING DEVICES
- ← LIGHTED ARROW PANEL
- TRAFFIC FLOW
- REFLECTS EXISTING SPEED LIMIT

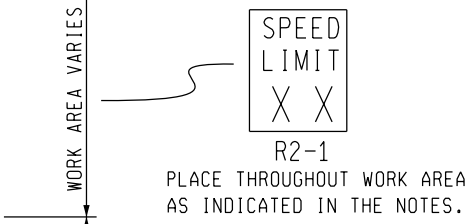
SIGN = 172 f+2 - TYPE B PLUS ADDITIONAL R2-1'S THROUGHOUT WORK AREA



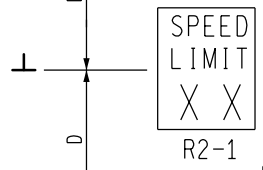
PLACE THIS SIGN ALONG WITH THE ADVANCE WORK ZONE SIGNING AS DEPICTED ON THE APPROPRIATE TYPICAL M0030a-M0080a.



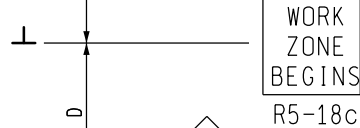
LEGEND REFLECTS SPEED LIMIT BEYOND WORK AREA



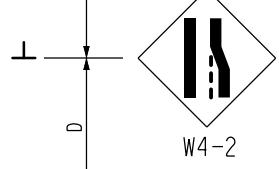
PLACE THROUGHOUT WORK AREA AS INDICATED IN THE NOTES.



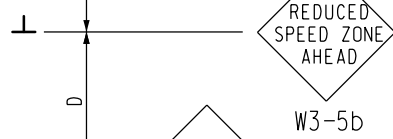
R2-1



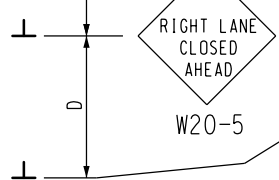
R5-18c



W4-2



W3-5b



W20-5



W20-1

↑ MAXIMUM 10MPH SPEED REDUCTION THIS DIRECTION

MDOT
Michigan Department of Transportation
TRAFFIC AND SAFETY
MAINTAINING TRAFFIC
TYPICAL

TYPICAL TEMPORARY TRAFFIC CONTROL FOR A ONE-LANE CLOSURE ON AN UNDIVIDED MULTI-LANE ROADWAY USING A SINGLE STEP DOWN IN SPEED LIMIT IN ONE DIRECTION ONLY

DRAWN BY: CON:AE:djf
CHECKED BY: BMM:CRB

OCTOBER 2011
PLAN DATE:

M0250a

SHEET
1 OF 2

NOT TO SCALE


NOTES

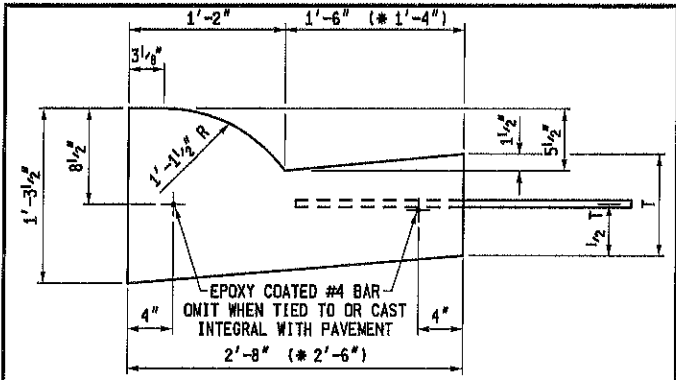
- 1B. D = DISTANCE BETWEEN TRAFFIC CONTROL DEVICES
 L = MINIMUM LENGTH OF TAPER
 B = LENGTH OF LONGITUDINAL BUFFER
 SEE **M0020a** FOR "D," "L," AND "B" VALUES
2. ALL NON-APPLICABLE SIGNING WITHIN THE CIA SHALL BE MODIFIED TO FIT CONDITIONS, COVERED OR REMOVED.
 3. DISTANCES BETWEEN SIGNS, THE VALUES FOR WHICH ARE SHOWN IN TABLE D, ARE APPROXIMATE AND MAY NEED ADJUSTING AS DIRECTED BY THE ENGINEER.
 - 3A. THE "WORK ZONE BEGINS" (R5-18c) SIGN SHALL BE USED ONLY IN THE INITIAL SIGNING SEQUENCE IN THE WORK ZONE. SUBSEQUENT SEQUENCES IN THE SAME WORK ZONE SHALL OMIT THIS SIGN AND THE QUANTITIES SHALL BE ADJUSTED APPROPRIATELY.
 - 4E. THE MAXIMUM RECOMMENDED DISTANCE(S) BETWEEN CHANNELIZING DEVICES SHOULD BE EQUAL IN FEET TO THE POSTED SPEED IN MILES PER HOUR ON TAPER(S) AND TWICE THE POSTED SPEED IN THE PARALLEL AREA(S).
 5. FOR OVERNIGHT CLOSURES, TYPE III BARRICADES SHALL BE LIGHTED.
 6. WHEN CALLED FOR IN THE FHWA ACCEPTANCE LETTER FOR THE SIGN SYSTEM SELECTED, THE TYPE A WARNING FLASHER, SHOWN ON THE WARNING SIGNS, SHALL BE POSITIONED ON THE SIDE OF THE SIGN NEAREST THE ROADWAY.
 7. ALL TEMPORARY SIGNS, TYPE III BARRICADES, THEIR SUPPORT SYSTEMS AND LIGHTING REQUIREMENTS SHALL MEET NCHRP 350 CRASHWORTHLY REQUIREMENTS STIPULATED IN THE CURRENT EDITION OF THE MICHIGAN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, THE CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION, THE STANDARD PLANS AND APPLICABLE SPECIAL PROVISIONS. ONLY DESIGNS AND MATERIALS APPROVED BY MDOT WILL BE ALLOWED.
 8. WHEN BUFFER AREAS ARE ESTABLISHED, THERE SHALL BE NO EQUIPMENT OR MATERIALS STORED OR WORK CONDUCTED IN THE BUFFER AREA.
 - 16A. ADDITIONAL SPEED LIMIT SIGNS REFLECTING THE REDUCED SPEED SHALL BE PLACED AFTER EACH MAJOR CROSSROAD THAT INTERSECTS THE WORK AREA WHERE THE REDUCED SPEED IS IN EFFECT, AND AT INTERVALS ALONG THE ROADWAY SUCH THAT NO SPEED LIMIT SIGNS REFLECTING THE REDUCED SPEED ARE MORE THAN TWO MILES APART.
 - 16B. WHEN REDUCED SPEED LIMITS ARE UTILIZED IN THE WORK AREA, ADDITIONAL SPEED LIMIT SIGNS RETURNING TRAFFIC TO ITS NORMAL SPEED SHALL BE PLACED BEYOND THE LIMITS OF THE REDUCED SPEED AS INDICATED.
 - 16E. WHEN EXISTING SPEED LIMITS ARE REDUCED MORE THAN 10 MPH, THE SPEED LIMIT SHALL BE STEPPED DOWN IN NO MORE THAN 10 MPH INCREMENTS.
 21. ALL EXISTING PAVEMENT MARKINGS WHICH ARE IN CONFLICT WITH EITHER PROPOSED CHANGES IN TRAFFIC PATTERNS OR PROPOSED TEMPORARY TRAFFIC MARKINGS, SHALL BE REMOVED BEFORE ANY CHANGE IS MADE IN THE TRAFFIC PATTERN. EXCEPTION WILL BE MADE FOR DAYTIME-ONLY TRAFFIC PATTERNS THAT ARE ADEQUATELY DELINEATED BY OTHER TRAFFIC CONTROL DEVICES.
 26. THE LIGHTED ARROW PANEL SHALL BE LOCATED AT THE BEGINNING OF THE TAPER AS SHOWN. WHEN PHYSICAL LIMITATIONS RESTRICT ITS PLACEMENT AS INDICATED, THEN IT SHALL BE PLACED AS CLOSE TO THE BEGINNING OF THE TAPER AS POSSIBLE.

SIGN SIZES

DIAMOND WARNING	- 48" x 48"
RECTANGULAR REGULATORY	- 48" x 60"
R5-18c REGULATORY	- 48" x 48"

NOT TO SCALE

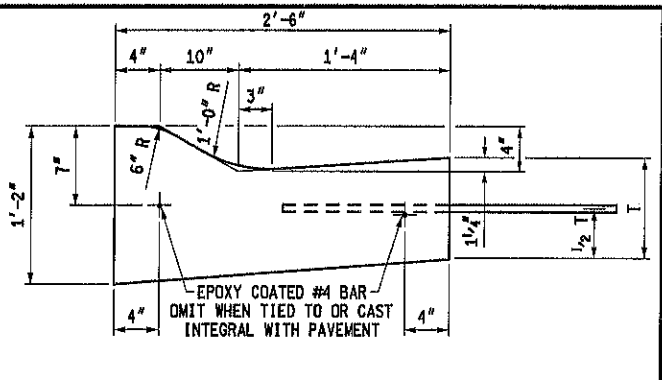
 TRAFFIC AND SAFETY MAINTAINING TRAFFIC TYPICAL	TYPICAL TEMPORARY TRAFFIC CONTROL FOR A ONE-LANE CLOSURE ON AN UNDIVIDED MULTI-LANE ROADWAY USING A SINGLE STEP DOWN IN SPEED LIMIT IN ONE DIRECTION ONLY
DRAWN BY: CON:AE:djf	OCTOBER 2011
CHECKED BY: BMM:CRB	PLAN DATE:
FILE: PW RD/TS/Typicals/Signs/MT NON FWY/M0250a.dgn	REV. 10/11/2011
M0250a	SHEET 2 OF 2



(* GUTTER PAN WIDTH MAY BE REDUCED WHEN APPROVED BY THE ENGINEER)

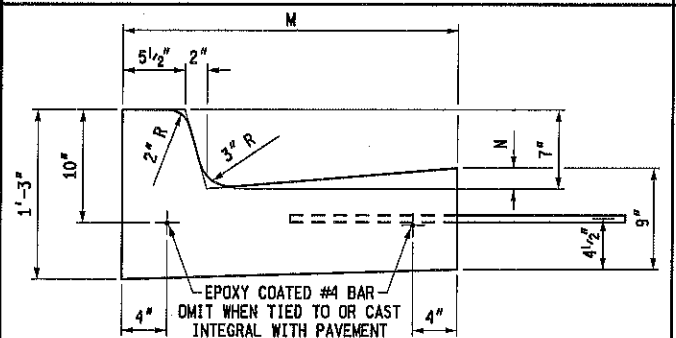
DETAIL	DIMENSION		LANE TIES	CONCRETE CYD / LFT	CONCRETE CYD / LFT
	T				
B1	9"		AS SHOWN	0.0900	(* 0.0855)
B2	9"		OMITTED	0.0900	(* 0.0855)
B3	10"		AS SHOWN	0.0941	(* 0.0894)

B



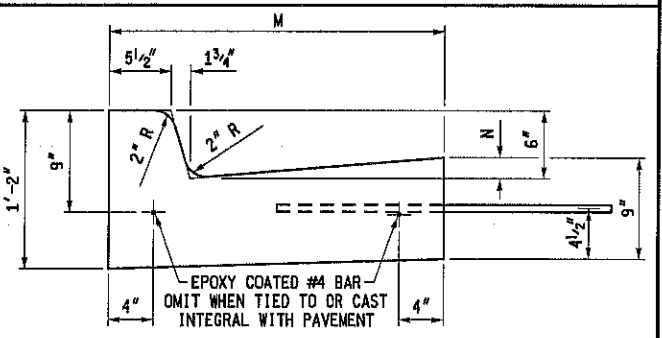
DETAIL	DIMENSION		LANE TIES	CONCRETE CYD / LFT
	T			
D1	9"		AS SHOWN	0.0788
D2	9"		OMITTED	0.0788
D3	10"		AS SHOWN	0.0826

D



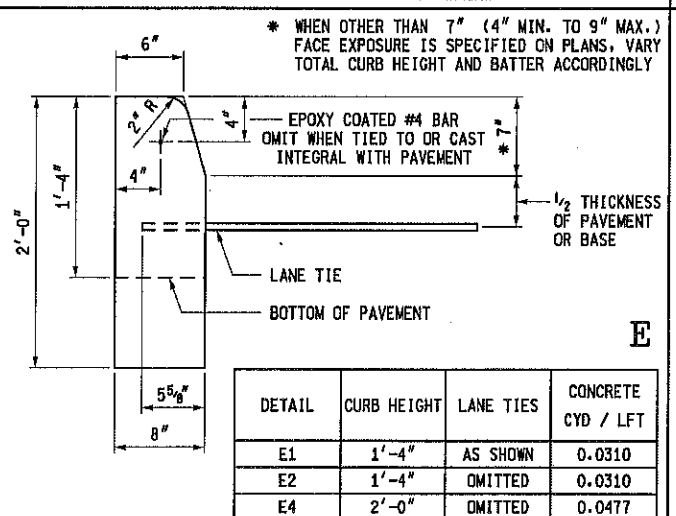
DETAIL	DIMENSION		LANE TIES	CONCRETE CYD / LFT
	M	N		
C1	1'-6"	1/8"	AS SHOWN	0.0506
C2	1'-6"	1/8"	OMITTED	0.0506
C3	2'-0"	1 3/8"	AS SHOWN	0.0632
C4	2'-0"	1 3/8"	OMITTED	0.0632
C5	2'-6"	1 7/8"	AS SHOWN	0.0757
C6	2'-6"	1 7/8"	OMITTED	0.0757

C



DETAIL	DIMENSION		LANE TIES	CONCRETE CYD / LFT
	M	N		
F1	1'-6"	1/8"	AS SHOWN	0.0484
F2	1'-6"	1/8"	OMITTED	0.0484
F3	2'-0"	1 3/8"	AS SHOWN	0.0610
F4	2'-0"	1 3/8"	OMITTED	0.0610
F5	2'-6"	1 7/8"	AS SHOWN	0.0737
F6	2'-6"	1 7/8"	OMITTED	0.0737

F



DETAIL	CURB HEIGHT	LANE TIES	CONCRETE CYD / LFT
E1	1'-4"	AS SHOWN	0.0310
E2	1'-4"	OMITTED	0.0310
E4	2'-0"	OMITTED	0.0477

E

MDOT
Michigan Department of Transportation

PREPARED BY
DESIGN DIVISION

DRAWN BY: B.L.T.

CHECKED BY: W.K.P.

DEPARTMENT DIRECTOR
Kirk T. Steudle

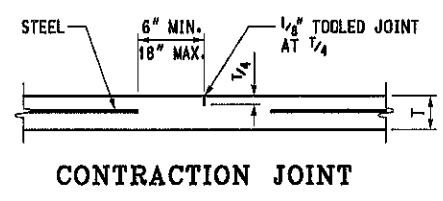
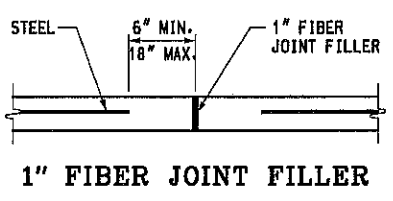
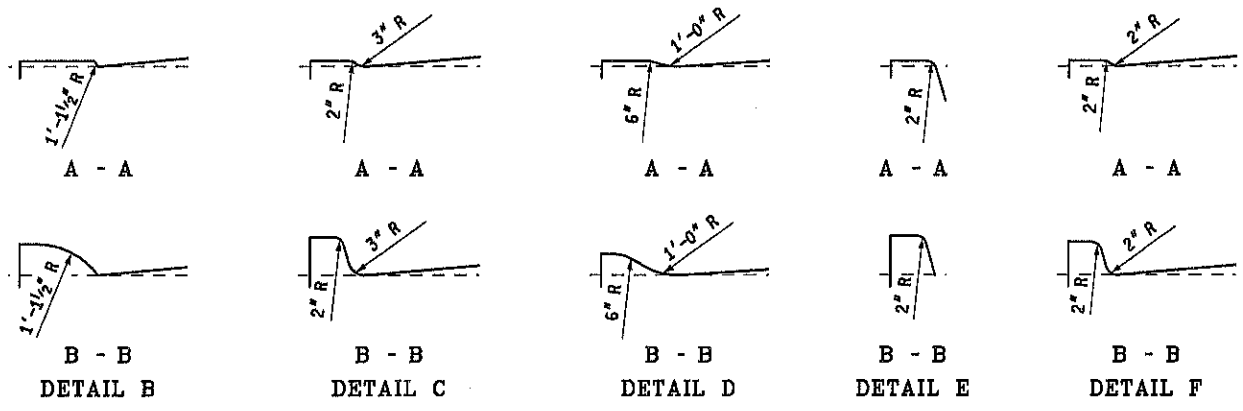
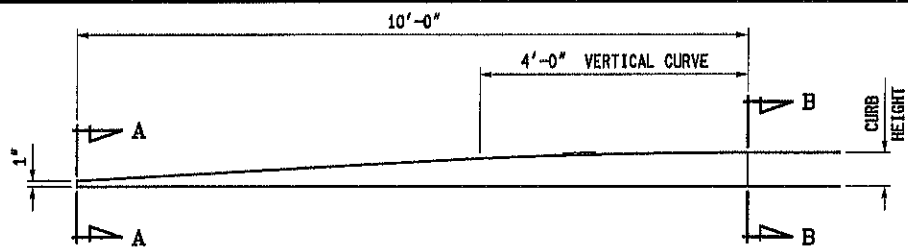
APPROVED BY: Randy Van Pelt
DIRECTOR, BUREAU OF FIELD SERVICES

APPROVED BY: Mark A. Van Pelt
DIRECTOR, BUREAU OF HIGHWAY DEVELOPMENT

MICHIGAN DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**CONCRETE CURB AND
CONCRETE CURB & GUTTER**

9-30-2014 F.H.W.A. APPROVAL	2-6-2014 PLAN DATE	R-30-G	SHEET 1 OF 2
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NOTES:

CURB AND GUTTER RADII SHALL BE DIMENSIONED TO THE FRONT EDGE OF THE GUTTER PAN OR EDGE OF PAVEMENT.

CONCRETE CURB AND GUTTER ENDINGS WILL BE PAID FOR IN LINEAR FEET OF THE ADJACENT CURB DETAIL.

JOINTS SHALL BE PLACED AT RIGHT ANGLES TO THE EDGE OF CONCRETE CURB AND GUTTER.

JOINTS DETAILED ON THE PLANS SHALL SUPERSEDE THOSE SPECIFIED ON THIS STANDARD PLAN.

BOTTOM SLOPE OF CURB AND GUTTER STRUCTURE MAY BE THE SAME SLOPE AS BOTTOM OF PAVEMENT. BACK OF CURB AND VERTICAL EDGE OF GUTTER PAN MAY HAVE A MAXIMUM 1/2" BATTER TO FACILITATE FORMING.

WHEN CURB AND GUTTER IS CAST INTEGRALLY, SEE CURRENT STANDARD PLAN R-31-SERIES.

ALL JOINTS FOR CURB OR CURB AND GUTTER ARE INCLUDED IN THE PAY ITEM FOR THE CURB OR CURB AND GUTTER.

JOINTS IN CURB OR CURB AND GUTTER NOT TIED TO CONCRETE PAVEMENT; ADJACENT TO CONCRETE BASE COURSE; OR ADJACENT TO HMA PAVEMENT:

- A. PLACE 1" FIBER JOINT FILLER AT 400' MAXIMUM INTERVALS.
- B. PLACE 1" FIBER JOINT FILLER AT SPRING POINTS OF INTERSECTING STREETS.
- C. PLACE 1/2" ISOLATION JOINT AT CATCH BASINS PER STANDARD PLAN R-37-SERIES.
- D. PLACE CONTRACTION JOINTS AT 40' MAXIMUM INTERVALS.

JOINTS IN CURB OR CURB AND GUTTER TIED TO JOINTED PAVEMENT

- A. PLACE 1" FIBER JOINT FILLER OPPOSITE ALL TRANSVERSE EXPANSION JOINTS IN PAVEMENT.
- B. PLACE 1/2" ISOLATION JOINT AT CATCH BASINS PER STANDARD PLAN R-37-SERIES.
- C. PLACE CONTRACTION JOINTS OPPOSITE ALL TRANSVERSE CONTRACTION JOINTS IN PAVEMENT.
- D. A SYMBOL (B) JOINT SHALL BE PLACED BETWEEN CURB OR CURB AND GUTTER AND ADJACENT CONCRETE PAVEMENT AS SPECIFIED ON STANDARD PLAN R-41-SERIES.

MICHIGAN DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**CONCRETE CURB AND
CONCRETE CURB & GUTTER**

9-30-2014 F.H.W.A. APPROVAL	2-6-2014 PLAN DATE	R-30-G	SHEET 2 OF 2
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SPECIFICATIONS FOR CATCH BASIN REPAIRS/REPLACEMENT

It is the intent of the Mason County Road Commission (MCRC) to receive bids for State Trunkline US-10 Catch Basin Repairs/Replacement. MCRC has the right to cancel the contract at any time if contractor performance is unsatisfactory.

The Board reserves the right to reject any or all bids, waive irregularities in any bid and make award in any manner deemed in the best interests of the MCRC.

GENERAL: The contractor shall comply with the intent of the General Requirements and Covenants, Division 1 of the Michigan Department of Transportation current "Standard Specifications for Construction", Section 403 Drainage Structures, and Details herein provided.

TIME LIMITATIONS: Prior to award of the contract, the contractor shall furnish a detailed Progress Schedule to the County for approval. Any variation from the Progress Schedule must have prior approval of the Engineer. Repairs/Replacement shall be started no later than ten (10) days after being awarded by the Mason County Road Commission.

REPAIR/REPLACEMENT:

1. Proposed work is as identified in the "Fix" column and will include all work and equipment necessary to complete as a Lump Sum item.
2. Sides and bottom of the catch basin shall be thoroughly flushed to remove leaves, sediment, gravel and all other debris.
3. Contractor shall complete vacuum water and debris from cleaned basin.
4. Large debris shall be removed by hand, if necessary, to attain a thoroughly cleaned catch basin.
5. Pink paint will be used to mark basins when cleaned.
6. All basins will be cleaned within 20 feet of curb face.

DIMENSIONS: Basins range in size from approximately four (4) to eight (8) feet in depth and two (2) to four (4) feet in width.

TRAFFIC CONTROL: The contractor will provide signing and traffic control for all lane closures and shoulder work in accordance with current MDOT Traffic Control Manual and the details herein provided. Payment is by Lump Sum and includes multiple closures as determined by the contractor and approved by the Engineer. Closure lengths shall not exceed one (1) mile in length per bound with a single closure permitted on each bound concurrently.

No lane closures shall be permitted on Friday 2:00 pm to Monday 7:00 am.

No work shall occur during the week of the July 4th holiday. No work shall occur during the Labor Day weekend from Friday 2:00 pm to Tuesday 7:00 am.

COMPLETION DATE: All work and cleanup shall be complete by September 20, 2018.

DEBRIS DISPOSAL: Documentation and weight tickets will be required by the Contractor for removal and disposal of all material gathered in accordance with current rules and regulations of governing agencies, Public Health, Natural Resources, and Transportation and turned over to the road commission. **No debris will be dumped on Mason County Road Commission property. NOTE: It is the contractor's responsibility to locate water and a place to legally discard water and materials cleaned from catch basins.**

DAMAGE: Any damage or accidents to public or private property **must** be reported immediately to Eric Moody, Superintendent.

SUPPLEMENTAL SPECIFICATIONS:

1. Provisions shall be made so that each place of business within any of the Project limits will have at least one driveway entrance open at all times.
2. Any request for a change in the contract must be made in writing to the Engineer at least 5 working days prior to the change being made.
3. All equipment is to be off the road at least one-half hour before dark.
4. There will be no Sunday work.
5. All construction equipment shall be equipped with working lights in accordance with the MDOT 2012 Standard Specifications for Construction.

CANCELLATION OF CONTRACT PROVISIONS: MCRC shall have the right to cancel the Contract for nonperformance/inadequate performance should an inspection by the designated representative reveal that the Contractor's performance and/or work results in any non-acceptable maintenance condition of one or all specified areas.

The designated representative at the time of the first circumstance shall call for a meeting with the contractor to issue a written warning of possible contract termination should the condition continue. If the condition should repeat itself a second time, written notice of termination shall be sent.

Submission of a bid will be interpreted as a conclusive presumption that the Contractor is thoroughly familiar with the bid requirements and specifications and that he/she understands and agrees to abide by each and all of the stipulations and requirements contained herein. The MCRC shall have the right to cancel the Contract between the Contractor and the MCRC for any violation as may be determined by the MCRC and such determination shall be the sole determination of the MCRC and its representatives.

ASSIGNMENT CLAUSE: The contract between the Mason County Road commission and the contractor may not be assigned to a third party without the written consent of the Mason County Road Commission.

QUESTIONS: MCRC Manager, Wayne A Schoonover at 231-757-2882, or wayne@masoncountyroads.com if you have questions

US-10 Catch Basin Repairs/Replacement